

Belleville Train Depot

Belleville, Wisconsin



Historic Structures Report & Study

November 25, 2015

Destree Design Architects, Inc.

DRAFT
For Review

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Introductions and Methods

This Historic Structures Report is prepared by Destree Design Architects Inc. on behalf of the Belleville Community Development Authority. The goal of this report is to assist in historical designation as well as guide the rehabilitation of the Historic Belleville Railroad Depot.

The property was surveyed on August 31, 2015 by Melissa Destree and Jason Ekstrom of Destree Design Architects. Field work was performed prior to selective demolition. This report should be revised with any findings discovered during demolition.

This report does not represent the results of detailed field investigation; it is an Historic Structures Report. This report offers the Village of Belleville guidance on how to pursue the restoration of this significant historic structure in accordance with the *United States Secretary of the Interior's Guidelines for Rehabilitation* and offer suggestions for the development of the site in context to its historical significance.

The Village of Belleville and the Community Development Authority has a unique opportunity to create an economic catalyst to strengthen and grow the community through the historic rehabilitation of the train depot. This depot rehabilitation will enhance local amenities like the Badger State Bike Trail and invigorate the districts redevelopment. In addition to this Historic Structures Report, the Village of Belleville has developed a Municipal Study (September 2015) with FEH Design to investigate the development potential of the rail corridor sites adjacent to the train depot.



Library Park with historic Village Hall, adjacent to Depot

History & Significance of the Belleville Train Depot



Victory Day Train arrives in Belleville - 1919

The Train Depot is a pivotal element in the historic context of Belleville's downtown. It is located adjacent and southeast of Library Park and the historic Village Hall. (The building and grounds of Library Park were placed on the *Department of Interior's National Register of Historic Places* in 1988.)

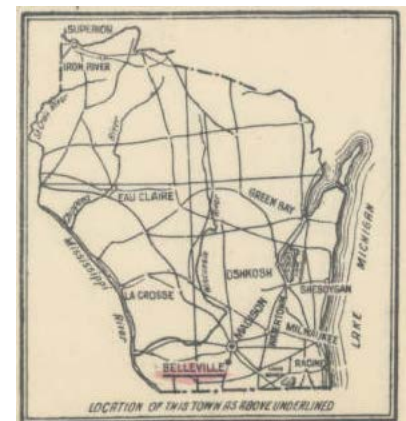
Train service began in Belleville in 1887, with the construction of the Chicago, Madison and Northern Railroad, (later named the Illinois Central). This line connected Freeport, IL, the Belleville area and Madison, WI. With the success of this line, in 1888, the freight and passenger depot was built at what is now East Pearl Street and South Park Street. The railroad business flourished, local businesses grew on Main Street and new housing began to be developed on the adjacent plated land. In 1892, residents ratified the incorporation of the Village of Belleville and the legislature granted their petition.

Construction

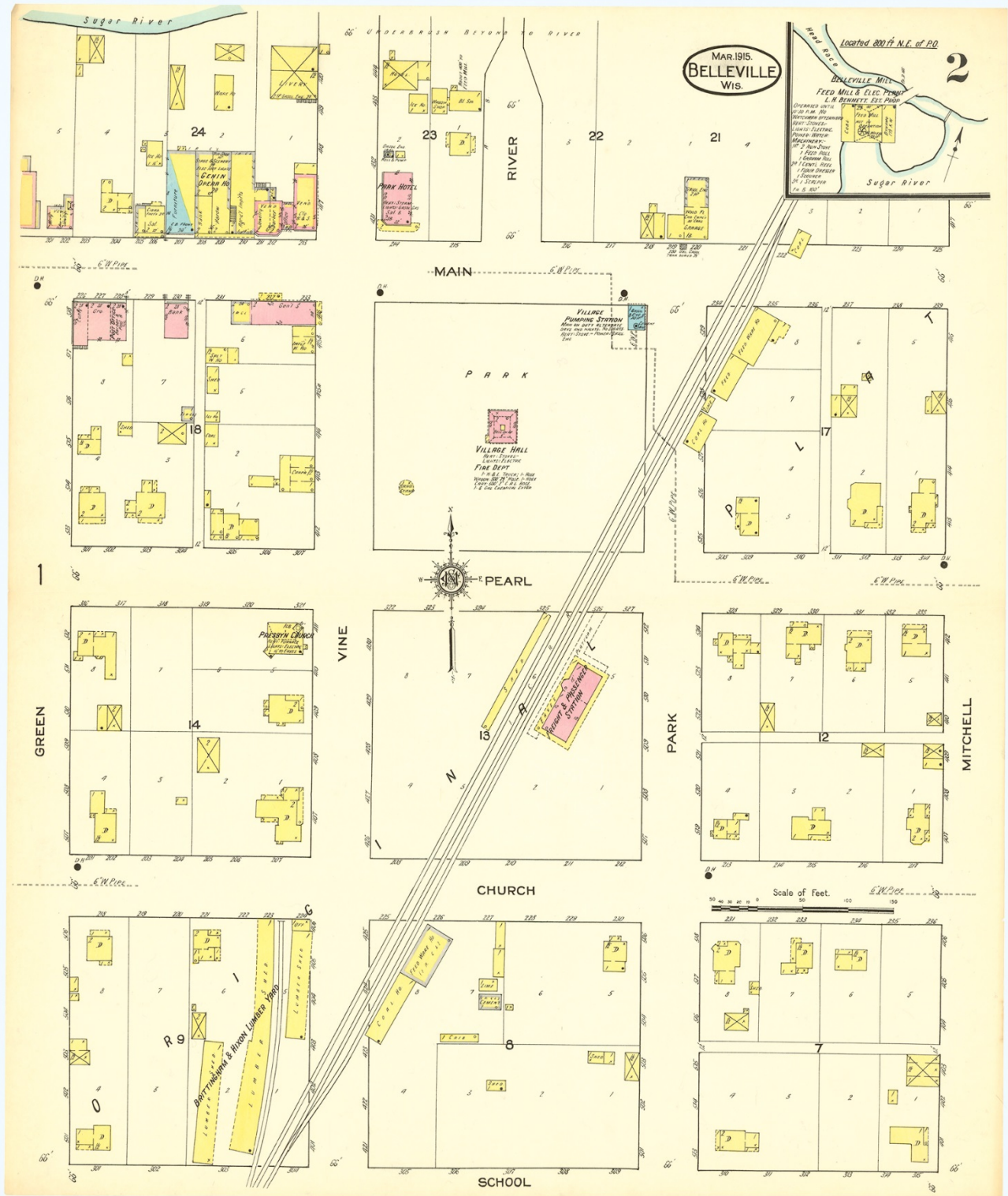
Original - 1888

Subsequent Alterations

1. Addition of toilets to the northeast facade after 1919 (c.1920's)
2. Wall damage on south brick wall was repaired mid-late 20th Century



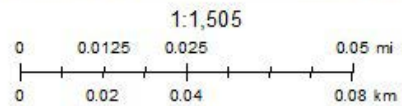
The depot and adjacent property is currently being used by Landmark Services Cooperative for an office and for fertilizer and grain storage. The property is adjacent to the historic Library Park and the historic Village Hall Building that is now used by the historical society. The current public library and post office are on adjacent properties in the rail corridor.





November 25, 2015

- CSM Text
- Tax Parcels



Site

Historically tracks dominated the landscape immediate to the depot. Historic maps and photographs show double tracks to the west of the depot building. Today the tracks have been removed and replaced with the Badger State Trail bike path. The Village of Belleville is currently determining how to best develop this rail corridor while enhancing this valuable community amenity.

Architectural Evaluation

The Belleville train depot is a stately example of an 1880's Masonry Stick Victorian. A transitional style linking Gothic Revival with Queen Anne. It showcases pitched roofs, gable ends, corbeled chimney, decorative trusses at gable ends, exposed detailed rafter tails and brackets. The passenger area has interior wooden wall cladding, plaster and stick work wood trim and details. The freight area features exposed redbrick walls, original pulley and freight doors with exposed structure. This structure is significantly contributing to both the history of the Village of Belleville, the economic growth of the community as well as the architectural style of the late 1880's. The historic structure is primarily intact, however lack of maintenance and water damage is causing irreversible damage of historic material. It is in the best interest of the community to move forward to preserve and rehabilitate this remarkable building.

Assessment of Exterior and Structure

The following are images of the existing building



North Facade – Gable Detail



East Facade



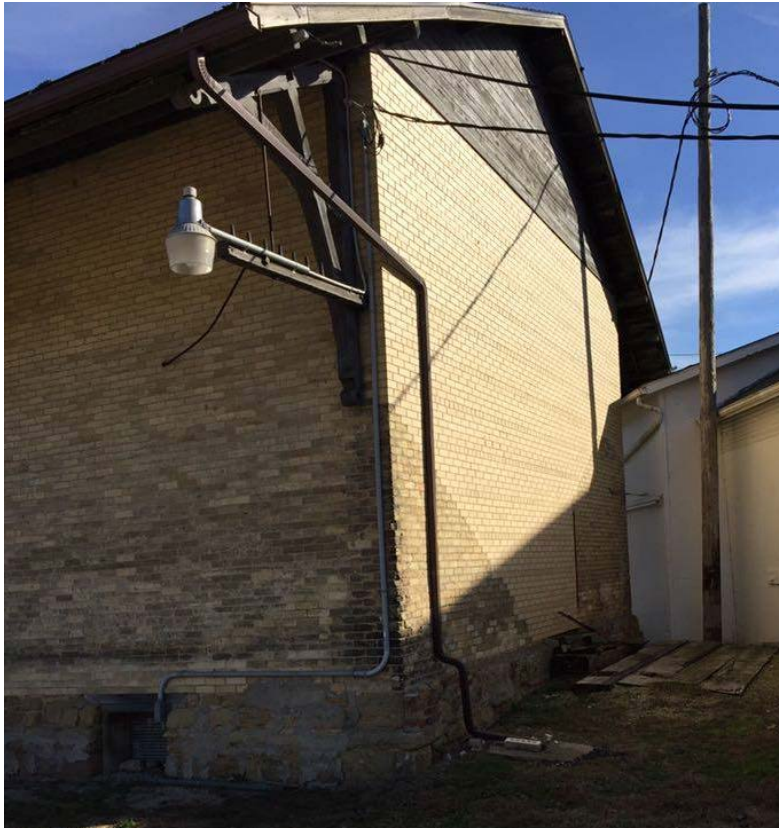
North Facade



North and West Facade



West Facade



South façade



East & North Facade

Materials & Elements Summary

Limestone foundation

Brick façade

Primary Exterior Watertown Brick

Expansion/Addition (toilet)

Brick Repair on South Façade (secondary façade)

Wood exterior trim –fascia, window trim

Substantial chamfered roof brackets, scalloped element, pierced with circular opening

Roofing material – non-historic asphalt

Historic Painted Brick Sign

Chimney – two original chimneys have been terminated below the roof.

Central corbelled chimney to be recreated above the roof line.

Track Platform – no longer existing, however there is evidence of location and slope

Exterior doors windows and transoms

Doors with transom

Baggage pulley access door



Original Pulley Door – West Façade



Interior at Pulley Door



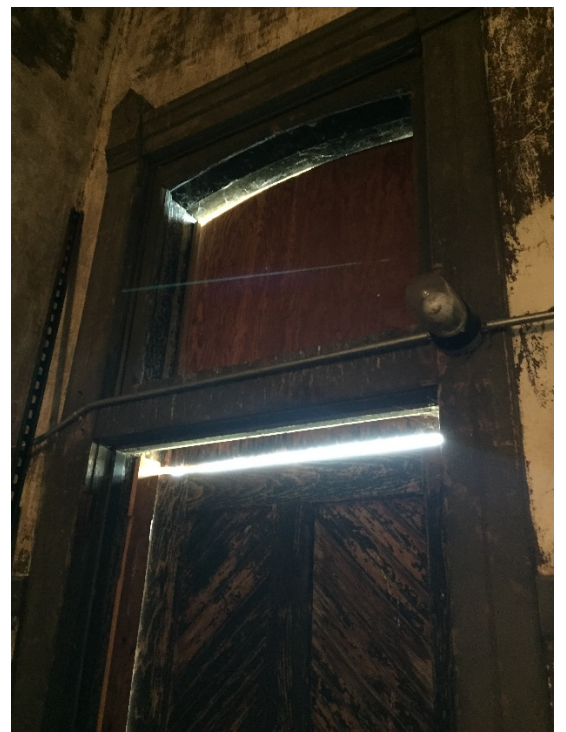
Original Freight Door



Original Passenger Entry Door



Original Window



Original Casing Trim

Construction Techniques

Masonry - Perimeter walls are triple-wythe of watertown exterior and common red clay interior brick approximately 13" in depth on stone rubble foundation. Masonry arches create window and door openings. There is general spalling, mortar deterioration around entire perimeter of structure. There is sever deterioration at the center of the east wall that needs to be immediately stabilized.

Roof Framing – Existing Slope is 6:12. The wood roof framing consists of clear span hand framed members with wood plank sheathing spanning the members approximately 16-18" on center. The truss members are constructed of 2x8 ceiling joists as bottom chords with 2x6 as diagonal web members. Visual inspection uncovers that some bottom chords are continuous, while a few chords are spliced at mid-span which is not an ideal situation. The bearing connection to the masonry is a 2-1/2" x 11 1/2" wood top plate. The wood roof planking exhibits signs of water damage from the leaking roof. There is deterioration, however it is in generally in good condition.



Brackets – The 5-1/2" x 5-1/2" wood brackets are inconsistently spaced around the structure, fastened to the masonry walls with bolt connections. These brackets support 3x6 wood purlins, 2x6 purlins and wood 1x4 tongue and groove planking. The 2x6 rafters span into the interior top chord to create overhang. An estimate of three 93) brackets may need to be recreated. This is a premier contributing historic feature.

Eaves – The edge condition of the roof system is deteriorated in areas and requires stabilization and selective replacement. Soffits show deterioration and require stabilization and selective replacement. As much as 80% may need replacement. This is caused by water infiltration over the years.

Floor Framing –The floor is heavy timber planking of various lengths. It is uneven and will not meet ADA accessibility tolerances. It is historically interesting, however the unevenness needs to be evaluated. The wood plank floor is as thick at full 2" x 9 ¾" on 3x12 wood joists, 16" on center, spanning 12ft. The joists sit on 12"x12" wood timbers spanning 8ft from wood columns (12x12) and the exterior masonry foundation walls. The freight and baggage area is at a higher floor level than the passenger waiting area. Non-historic framing modifications have occurred over time at the middle of the span. A second layer of floor framing was installed over the original 'office area' to align with the floor level of the baggage and freight area. Generally, the condition of the framing is good, non-historic elements require evaluation and supplementation based on the reuse of the depot.

Basement and Foundation – the existing basement is exposed limestone wall with 5-6ft in head clearance, water seepage is exhibited through the foundation. There appears to be a solid compacted or concrete floor. Access is from the exterior scuttle stair; there is no interior stair. The foundation requires removal of non-historic mortar patches, new mortar repair, partial stone inserts and tuck pointing.



Basement crawl space with limestone foundation

Platform – currently not existing. There is evidence of existing location on masonry wall



Trackside

Significance of Structural System – Typical Heavy Timber Framing and Masonry bearing wall typical to the style and time period.

Chronology of Alterations

1. Toilet Room addition – late 1920's – 1930's (poured foundation)
2. South Wall brick replacement/ repair in the mid-late 20th Century

Capacity of Structural Support

First Floor Framing – visual inspection concludes that minimal modification will be needed to stabilize the structure to maintain a 100 lbs/sf for assembly use. Reinforcement will be required for equipment that has weights above 100 lbs/sf

Roof Framing – with repair and stabilization the roof framing will meet snow drifting and roof loading requirements.

Contributing Features – Brackets with Telegraph wire apparatus



Historical markings



Assessment of Interior Features

Materials

Walls above the window sill level are plaster over sawn lath

Walls below are hard pine (confirm) wainscot, tongue and groove

Walls in the baggage room are exposed brick with significant historical markings

Interior Doors and Transoms – Stick Victorian

Evidence of benches to be investigated

Door hardware - enameled knob

Paint analysis to be preformed



Passenger Area

Room Evaluation

Passenger Area - Wall materials are hard pine wainscot with plaster walls and Stick Victorian casing, trim and doors. Wood flooring is damaged. This floor area is 20" lower than the ticket and freight areas.

Ticket Office – this area has been renovate with paneling, small window inserts, OSB and acoustical ceiling tile. There is evidence of the original materials.

Water Closets – these are non-operational. This area was added to the structure in the 1920's. This addition is non-contributing and should be removed.

Freight Area – Exposed red brick with Historical markings and etchings cover walls. Exposed structure. The original pulley door and freight door is in place



Freight Area with Pulley Door

Building Systems description

Historic – evidence of systems

Electrical & Lighting – Historic wiring not in place. This system has been modified for use as storage shed and office.

Heating – evidence of original stove location at passenger/office chimney location, no stoves existing

Plumbing – two water closets appear to have been added to the original structure sometime after 1919, assuming 1920's due to the poured concrete foundation. Non-operational.



Existing Water Closet

Existing Conditions

Assessment of Exterior Conditions - Damage

Roofing - asphalt roofing is delaminated and missing in sections allowing water into to the structure. There are two non-historic skylights that need to be removed.

Foundation - The foundation requires removal of non-historic mortar patches, new mortar, partial stone inserts and tuck pointing required. Windows in foundation need stabilization and repair.

Brick Degradation – Triple Wythe wall is deteriorating from surface water and leaking roof allowing water into wall cavity. Tuck pointing is needed on all facades

Eaves & Soffits – In disrepair due to water damage and impact damage



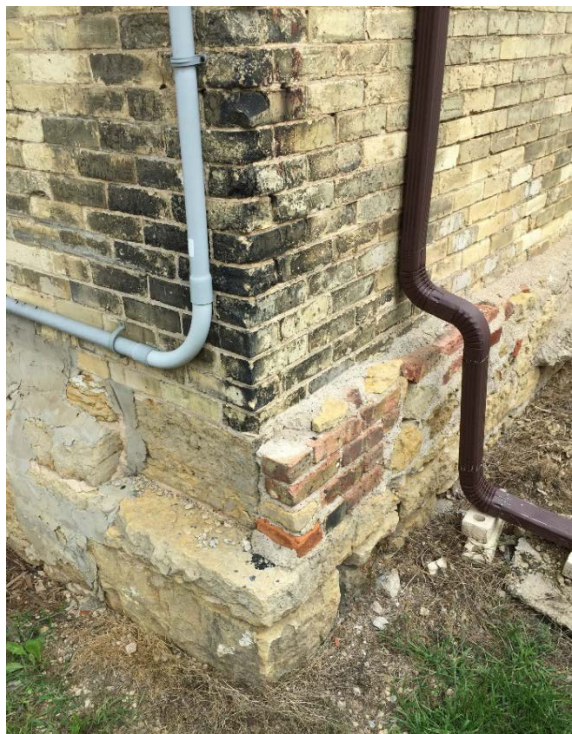
Eaves



Soffits



Water damage and degraded masonry



Mortar analysis –will be performed in preparation for repair



South wall – Non-historic Brick Condition, Exterior



South wall – Non-historic Brick Condition, Interior

Prioritize repair and stabilization

- Phase 1:** Prioritize enclosure of structure to prevent further water damage
Stabilize Roof and Structure
Stabilize Brick and tuck point

Refer to Architectural Drawings for additional information

Assessment of Materials Conservation

Preserve and Restore the envelop of the structure

- Roofing – Replace with split shake roofing
- Toilet room addition – remove this non-contributing element
- Windows – Restore original, historic replacements for missing windows
- Telegraph apparatus
- Brackets and Trim
- Brick and Masonry

Rehabilitate and Renovate Interior

- Retain original historic 1888 trim, doors, etc.
- Rehabilitate interior for new retail/restaurant use
- Maintain wall treatments in original areas
- Maintain volume and sense of historic space

Treatment and Use Statement:

Primary Facades – North, East & West will be stabilized and preserved.

Secondary Facade – the South façade has been repaired in the mid-late 20th Century. It has no window openings and only a 1 ft overhang. Based on this non-historic brick condition, this façade would be an ideal location if an historically sensitive addition is needed for the re-use of the building.

Removal - 1920's Toilet room addition – It is recommended to remove this non-contributing element

Reconstruction – It is recommended to reconstruct the track platform based on building location markings and site evidence.



East Side Frieght Door

All work should follow the Secretary of the Interior's Standards for Rehabilitation. As listed below:

The Secretary of the Interior's Standards for Rehabilitation

The Standards (Department of Interior regulations, 36 CFR 67) pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and the interior, related landscape features and the building's site and environment as well as attached, adjacent, or related new construction. The Standards are to be applied to specific rehabilitation projects in a reasonable manner, taking into consideration economic and technical feasibility.

- 1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.*
- 2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.*
- 3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.*
- 4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.*
- 5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.*
- 6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.*
- 7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.*
- 8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.*
- 9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.*
- 10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.*

Preservation Briefs provide detailed specifications and guidance for repair and stabilization for the Belleville Train Depot including but not limited to:

- No. 2 – Repointing Mortar Joints
- No. 9 – The Repair of Historic Windows
- No. 10 – Historic Paint
- No. 19 – Replacement of Wooden Shingles
- No. 21 – Repairing Flat Plaster Walls and Ceilings

Building Code and Accessibility Impacts

Building Footprint = 2295 sf (+75 sf Toilet addition)

Type III Construction

Future Re-Use

If the building becomes an Assembly Use (restaurant with a seating capacity greater than 49 persons). 2 exits will be required. Under 49 persons, the project falls under the 'Business' use.

Fire Sprinkler System

Fire protection sprinkler system should be evaluated based on re-use. A sprinkler system will be required for a restaurant capacity over 100 persons.

Accessibility

The building, with minor modifications, will be able to fully accommodate the guidelines for Accessibility. Reconstruction of the track side platform allows for a sensitive ADA ramp solution to allow for access to both levels of the train depot.

Toilet Room Accessibility

New fully accessible toilet rooms will be needed to meet re-use, once the project is defined. The following are guidelines for a restaurant use.

Capacity of 15	(code - single toilet)
Capacity of 16-49	(code - toilet room for each sex)
Capacity of 50-99	(recommend Two toilets for each sex)
Capacity of 100	(code – two toilets per sex)

Proposed Work

The following are recommendations for proposed work based on existing conditions, rehabilitation and preservation objectives for the shell of the building.

Architectural [see drawing diagram]

- New Roofing – historically appropriate roofing material is recommended if funds become available to support this scope of work. Split shake.
- Repair Brick and Tuck point
- The Platform Deck on the 'track' side of the depot should be recreated
- Window restoration on existing windows (Preservation Brief No.9) New historically appropriate windows where no historic material is found.
- Gutters & downspouts
- Repair damaged eaves
- Wood floors – stabilize and apply Tung Oil
- New Toilet facilities – ADA accessible
- Entry – ADA accessible. Integrate with reconstructed trackside platform
- Insulation – roofing and rim joist only.
 - Not recommended by NPS for masonry walls
- Remove non-historic concrete east side loading dock
- Roof framing – the team will evaluate if the freight and baggage space should remain raw and unfinished as original.

Conservation & Preservation

- Retain historical markings and signage on interior and exterior
- Paint Historic colors (Preservation Brief No. 10)

Mechanical/Electrical

- Preserve telegraph apparatus
- New Historically sensitive modern heating systems, air conditioning, dehumidification and humidification.

Site

- Raise grade for proper drainage
- Recreate the Platform
- New Parking lot
- Landscaping

Cost Estimate for Shell Restoration and Preservation

Belleville Train Depot					11/25/2015
Exterior Shell Preservation & Stabilization					
Scope		Unit		Cost	Total
Demolition					
	Selective Demolition	2295		\$ 13.00	\$ 29,835.00
					Shell Only, remove Loading Dock
					\$ 29,835.00
Roofing					Remarks
	Split Red Shakes	4300	sf	\$ 4.50	\$ 19,350.00
	Fire Retardant	4300	sf	\$ 0.72	\$ 3,096.00
	Underlayment, Ice & Water	4300	sf	\$ 1.25	\$ 5,375.00
	Sheathing	4300	sf	\$ 2.52	\$ 10,836.00
	Insulation	2300	sf	\$ 6.50	\$ 14,950.00
					\$ 53,607.00
Eaves					
	Copper Gutters/Dwospouts	252	lin	\$ 25.00	\$ 6,300.00
	Eaves/Soffit Repair	2000	sf	\$ 13.75	\$ 27,500.00
	Brackets	22	ea	\$ 1,100.00	\$ 24,200.00
					Repair and replacement, Paint
					Preserve, Repair and Paint
					\$ 58,000.00
Openings					Exterior Only - Includes Paint
	Large Historic Windows	11	ea	\$ 4,000.00	\$ 44,000.00
	Transom Historic Windows	6	ea	\$ 2,000.00	\$ 12,000.00
	Exterior Door	2	ea	\$ 2,500.00	\$ 5,000.00
	Pulley Door	1	ea	\$ 1,800.00	\$ 1,800.00
	Frieght Door	1	ea	\$ 4,000.00	\$ 4,000.00
					Restore or Historic Replacement
					Restore or Historic Replacement
					Restore or Historic Replacement
					Restore
					2 doors
					\$ 66,800.00
Structural		1584	lin	\$ 10.00	\$ 15,840.00
					Sister or replace selective members
					\$ 15,840.00
Stone & Brick					
	Foundation Repair	450	sf	\$ 5.32	\$ 2,394.00
	Crawl Space Tuck Piont & Repair	1600	sf	\$ 5.32	\$ 8,512.00
	Brick Cut & Repoint	3100	sf	\$ 6.22	\$ 19,282.00
	Brick Cleaning	3100	sf	\$ 2.00	\$ 6,200.00
	Remove & Replace	600	ea	\$ 29.00	\$ 17,400.00
					\$ 53,788.00
Site Work					
	Tentative Grading	-	-	\$ -	\$ 20,000.00
					Allowance
Interior Work					NOT Included
Mechanical Work		2295	sf	\$ 2.50	\$ 5,737.50
Plumbing					Temporary
Electrical		2295	sf	\$ 2.00	\$ 4,590.00
Fire Protection					NOT Included
					Temporary - No Service Upgrade
					NOT Included
					\$308,197.50
					Sub-Total Shell Construction
Contingency		15%			\$ 46,229.63
General Conditions		12%			\$ 36,983.70
Profit & Overhead		10%			\$ 30,819.75
					\$422,230.58
					Total Shell Construction
No ADA access improvements					
No Site Improvements					
No Interior Work					

Appendices

Bibliography

Studies - Village of Belleville

2007 Project Report for the Village of Belleville Landmarks Services Relocation Feasibility Study, MSA Professional Services, March 21, 2007.

2008 Belleville Depot Report, MSA Professional Services, May 20, 2008.

2015 Municipal Study – Village of Belleville, Wisconsin
FEH Design, September 17, 2015

Historic Images

Belleville Historical Society

Belleville Area Historical Society

2001, 'A History of Belleville and Surroundings', A Publication of the Belleville Area Historical Society

Drawings

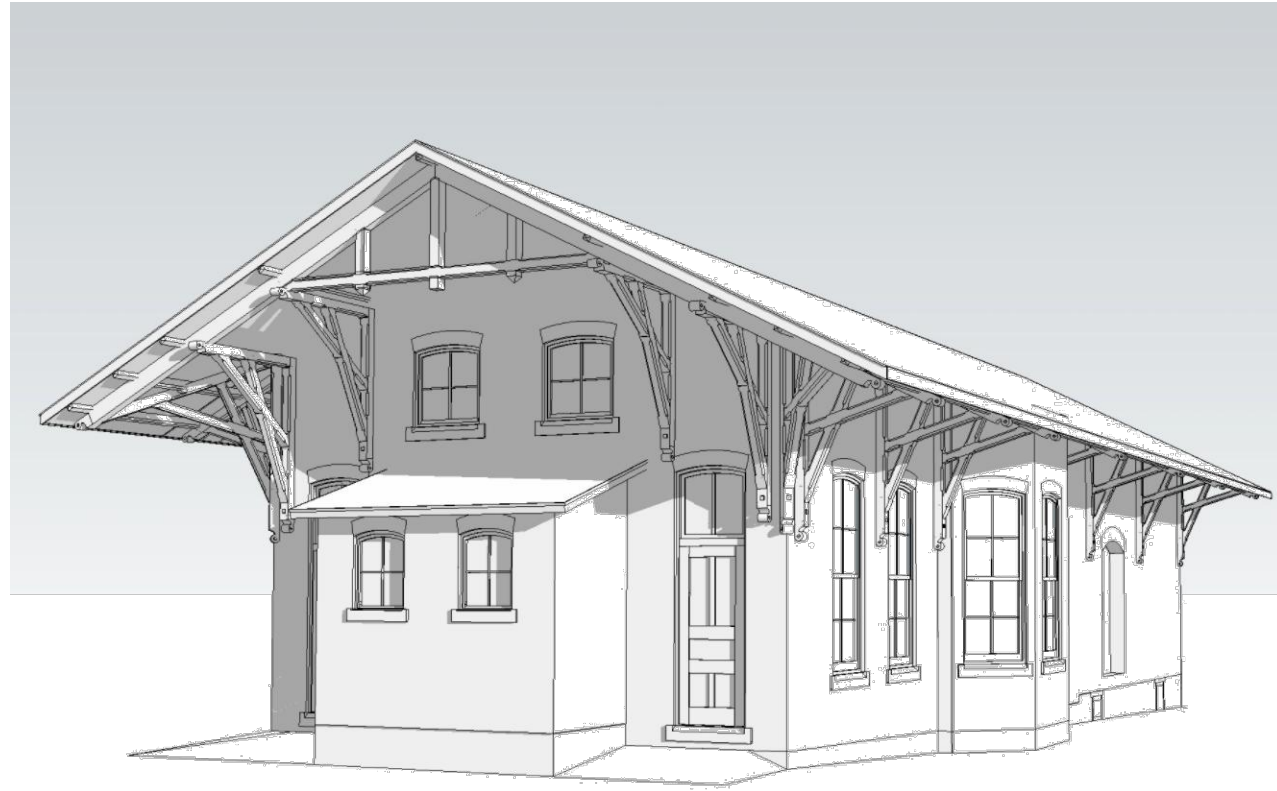
Drawings of Existing Conditions

Drawing Visualizing Historic Structure

Drawing of Proposed Rehabilitation Options

Existing Photographs





1 RENDERING
NOT TO SCALE

BELLEVILLE TRAIN DEPOT

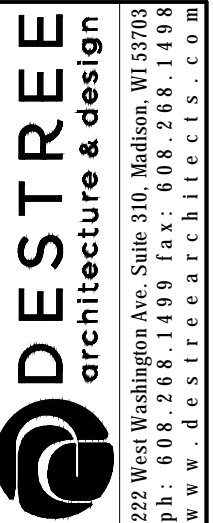
HISTORIC STRUCTURES REPORT & STUDY

NOVEMBER 25, 2015

SOUTH PARK STREET/EAST PEARL STREET

BELLEVILLE, WI

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SYMBOL LEGEND:

- XX INTERIOR ELEVATION CALLOUT
- XXXX DETAIL CALLOUT
- XTG WALL
- NEW WALL
- EXISTING DOOR

SQUARE FOOTAGES

EXISTING BUILDING FOOTPRINT = 2295+/- SF
 TOILET= 75 SF

SHEET INDEX:

TITLE

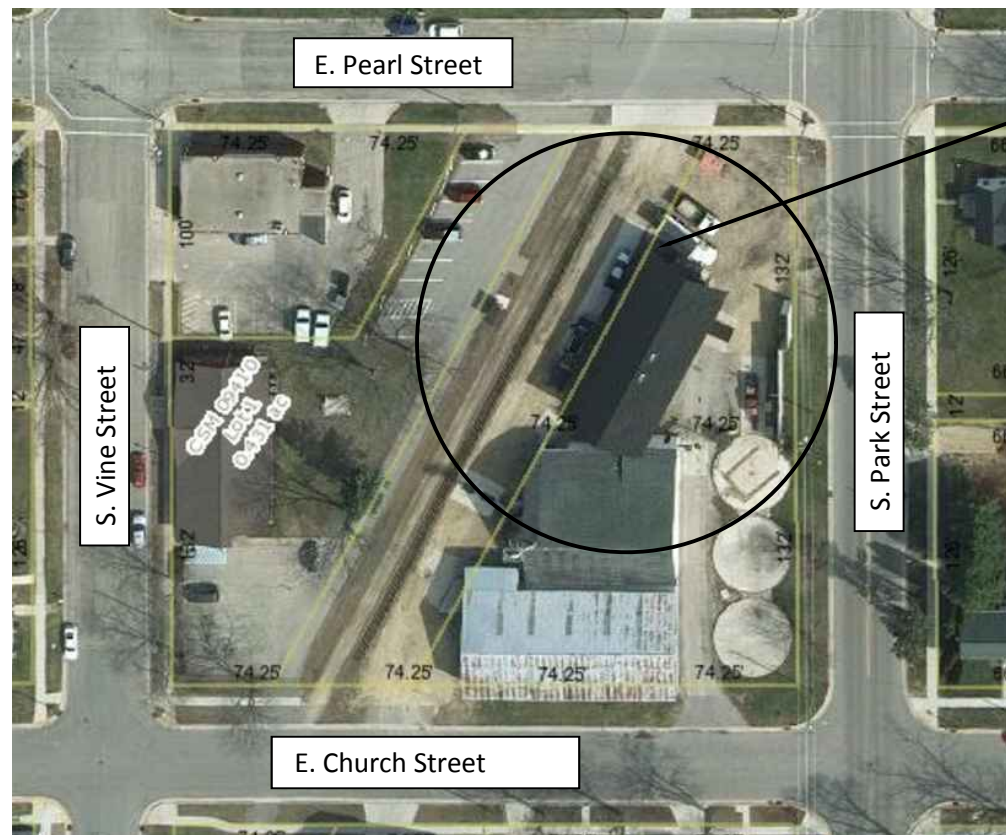
T000 COVER

ARCHITECTURAL

- A000 CRAWL SPACE PLAN
- A100 EXISTING FLOOR PLAN
- A101 EXISTING ROOF PLAN
- A300 EXISTING EXTERIOR ELEVATIONS
- A301 EXISTING EXTERIOR ELEVATIONS
- A302 EXISTING EXTERIOR ELEVATIONS
- A303 EXTERIOR ELEVATIONS
- A304 EXTERIOR ELEVATIONS
- A305 EXISTING 3D VIEWS
- A900 PLAN OPTIONS
- A901 PLAN OPTIONS

DEFINITIONS:

- TYP - TYPICAL
- XTG - EXISTING



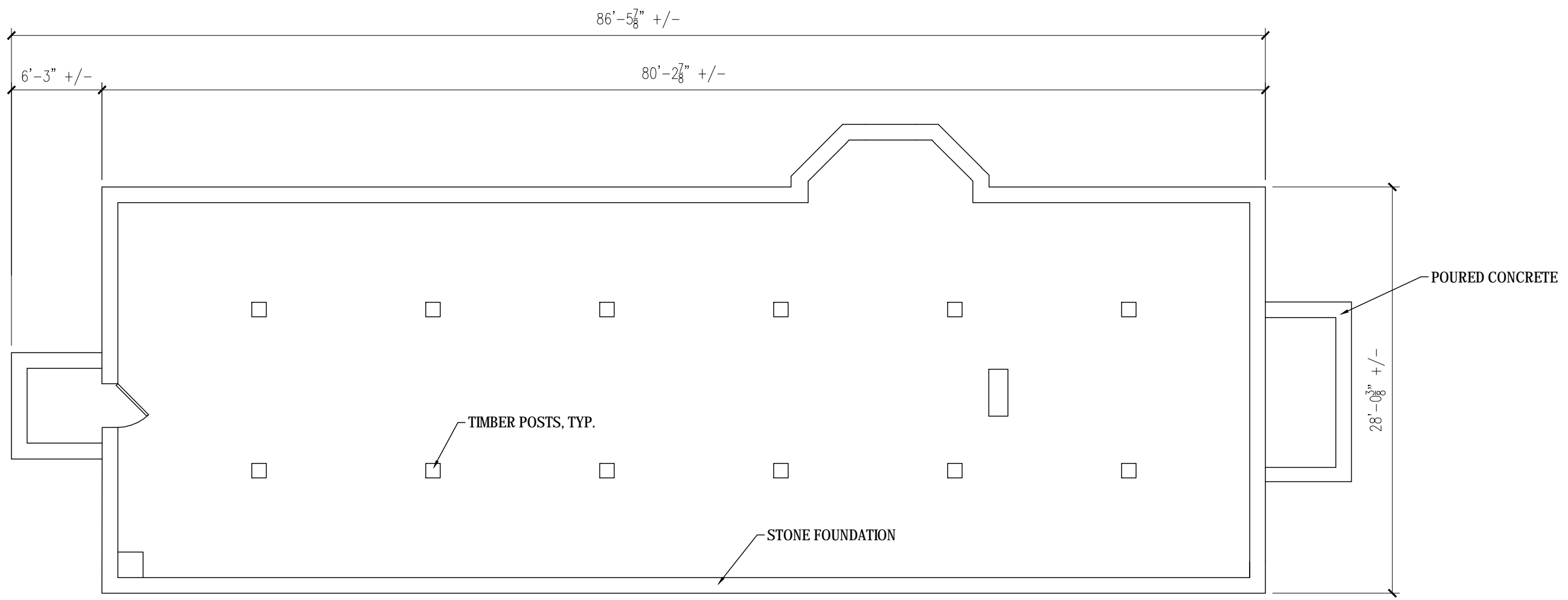
SITE LOCATION

2 SITE MAP
NOT TO SCALE

BELLEVILLE TRAIN DEPOT
 HISTORIC STRUCTURES REPORT & STUDY
 SOUTH PARK STREET/EAST PEARL STREET
 BELLEVILLE, WI

PROJECT:	
NUMBER:	150415.00
DATE:	11.25.15
ISSUANCES:	
---	11.25.15
REV:	---

SHEET:
T000



1 EXISTING CRAWLSPACE PLAN
1/8" = 1'-0"

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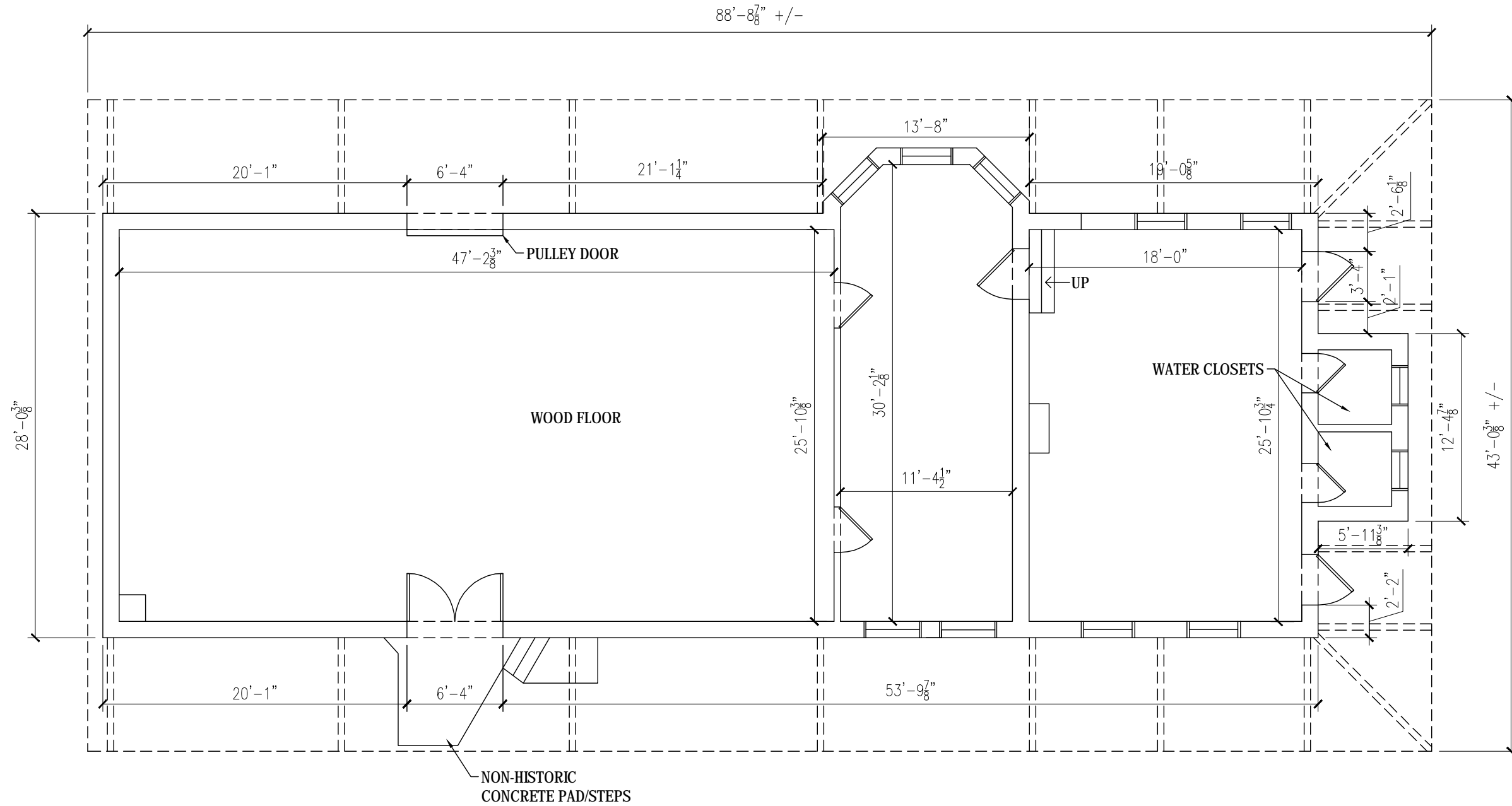
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1 EXISTING FLOOR PLAN
1/8" = 1'-0"

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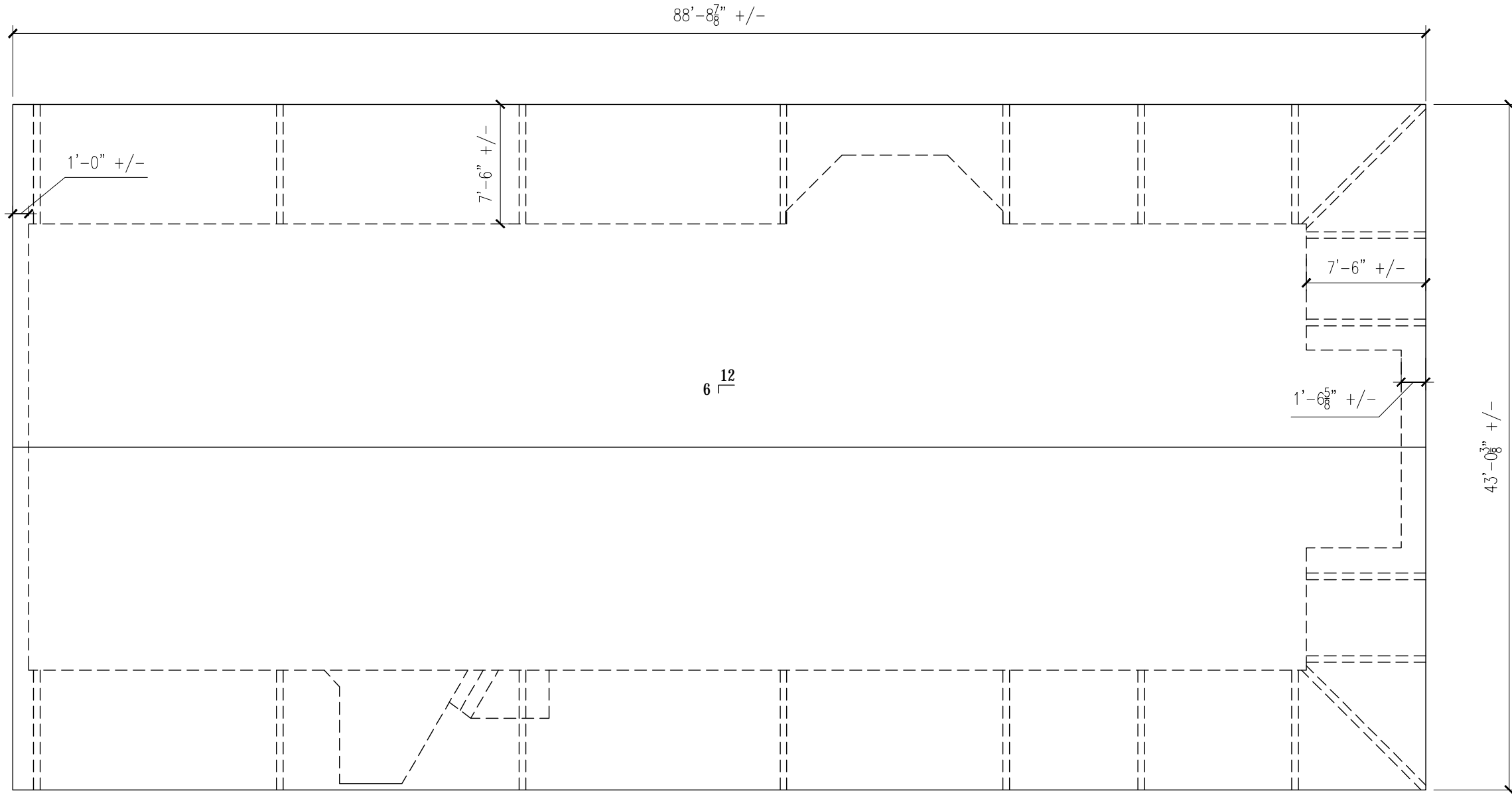


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PROJECT:	
NUMBER:	150415.00
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SHEET:	

A100




1 ROOF PLAN
 1/8" = 1'-0"

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ISSUANCES:	
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SHEET:	

A101

ROOF MAINTENANCE HAS BEEN NEGLECTED- LEAKING WATER HAS STARTED TO COMPROMISE THE STRUCTURAL BRACKETS



AREA # 2- ROOF OVERHANG CONDITION



ROOF MAINTENANCE HAS BEEN NEGLECTED- WATER HAS BEEN LEAKING INTO THE SOFFIT AND HAS ROTTED THE EXISTING WOOD BREADBOARD THIS IS A TYPICAL PROBLEM AROUND THE STRUCTURE



MAINTENANCE TUCK POINTING WAS NEGLECTED

AREA # 1- BRICK / STONE DETERIORATION @ BASE

ROOF WATER SPLASHING OFF BLACKTOP ALONG WITH FREEZE AND THAW CYCLES HAS BROKEN DOWN THE BRICK

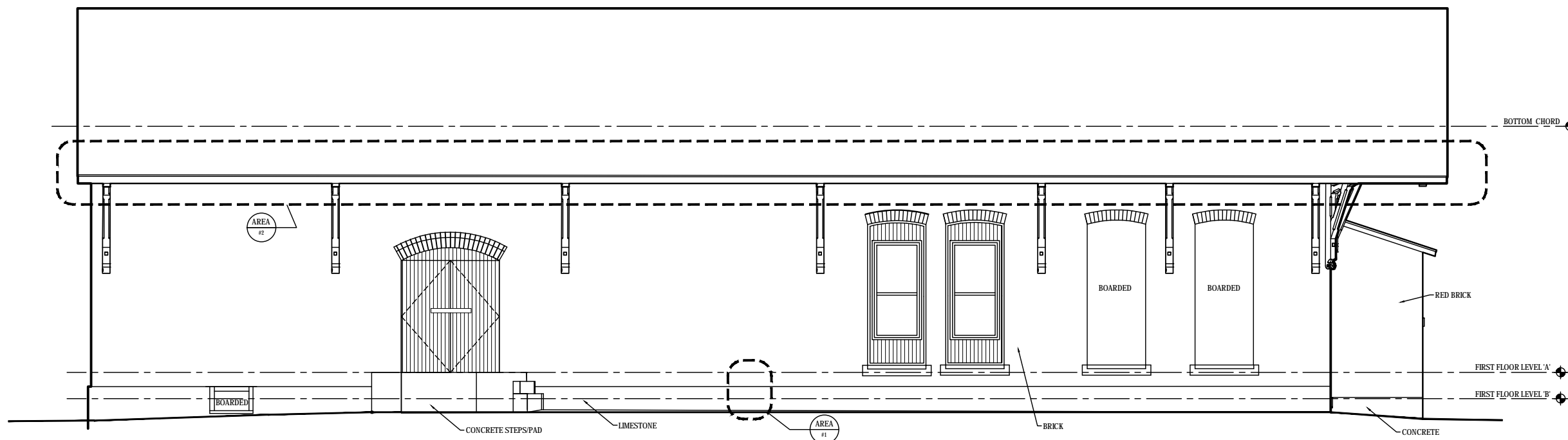
MAINTENANCE TUCK POINTING WAS NEGLECTED

ROOF WATER SPLASHING OFF BLACKTOP ALONG WITH FREEZE AND THAW CYCLES HAS DETERIORATED THE BRICK POWDER

ROOF LEAKS ALLOWING WATER TO PENETRATE WALL SYSTEM



LIMESTONE FOUNDATION SHOWING THE SAME DETERIORATION IN CERTAIN AREAS



1 EXISTING SIDE ELEVATION
1/8" = 1'-0"

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BELLEVILLE TRAIN DEPOT
HISTORIC STRUCTURES REPORT & STUDY
SOUTH PARK STREET/EAST PEARL STREET
BELLEVILLE, WI

PROJECT:	
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DATE:	11.25.15
ISSUANCES:	
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REV:	---
SHEET:	

A300

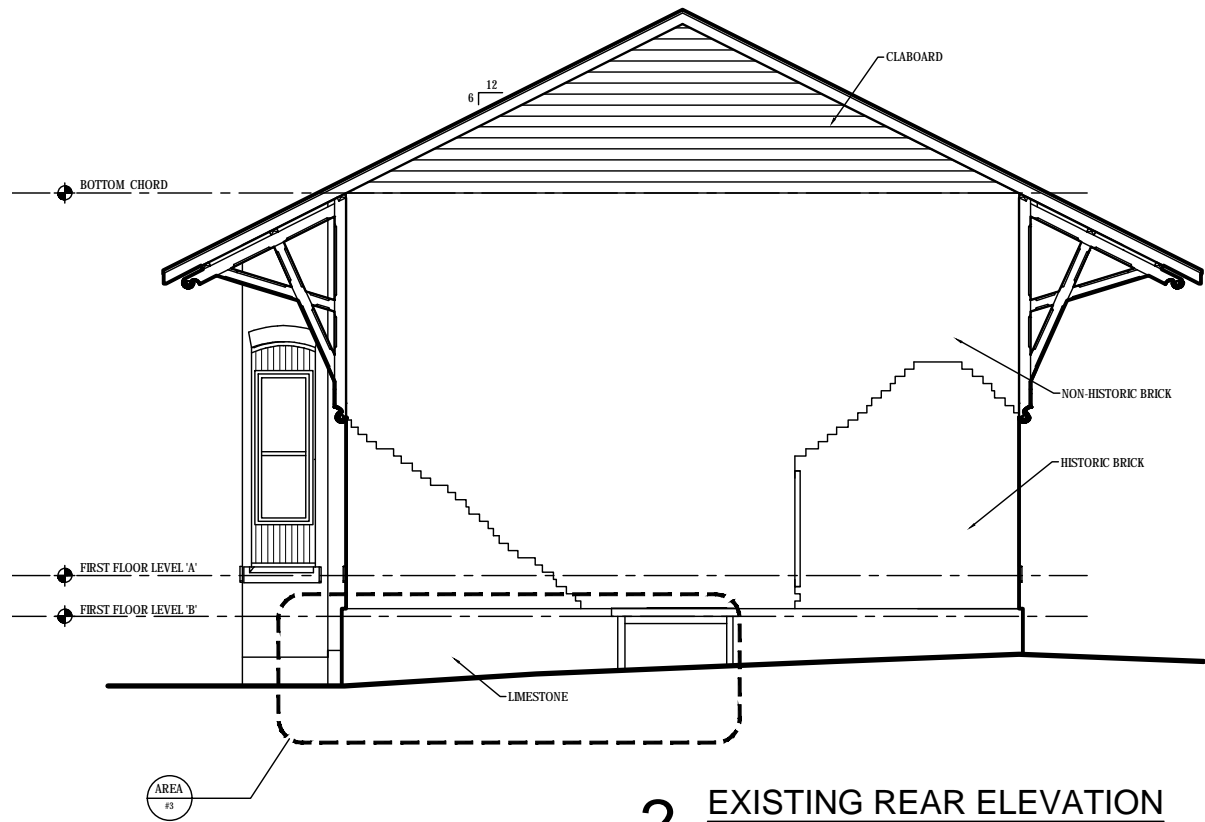


MISCELLANEOUS BRICK USED FOR REPAIRS REPLACE W/ LOCAL LIMESTONE

INCORRECT MORTAR USED FOR REPAIRS

EXISTING CRAWLSPACE ACCESS BURIED WITH RAILROAD TIMBERS

AREA #3



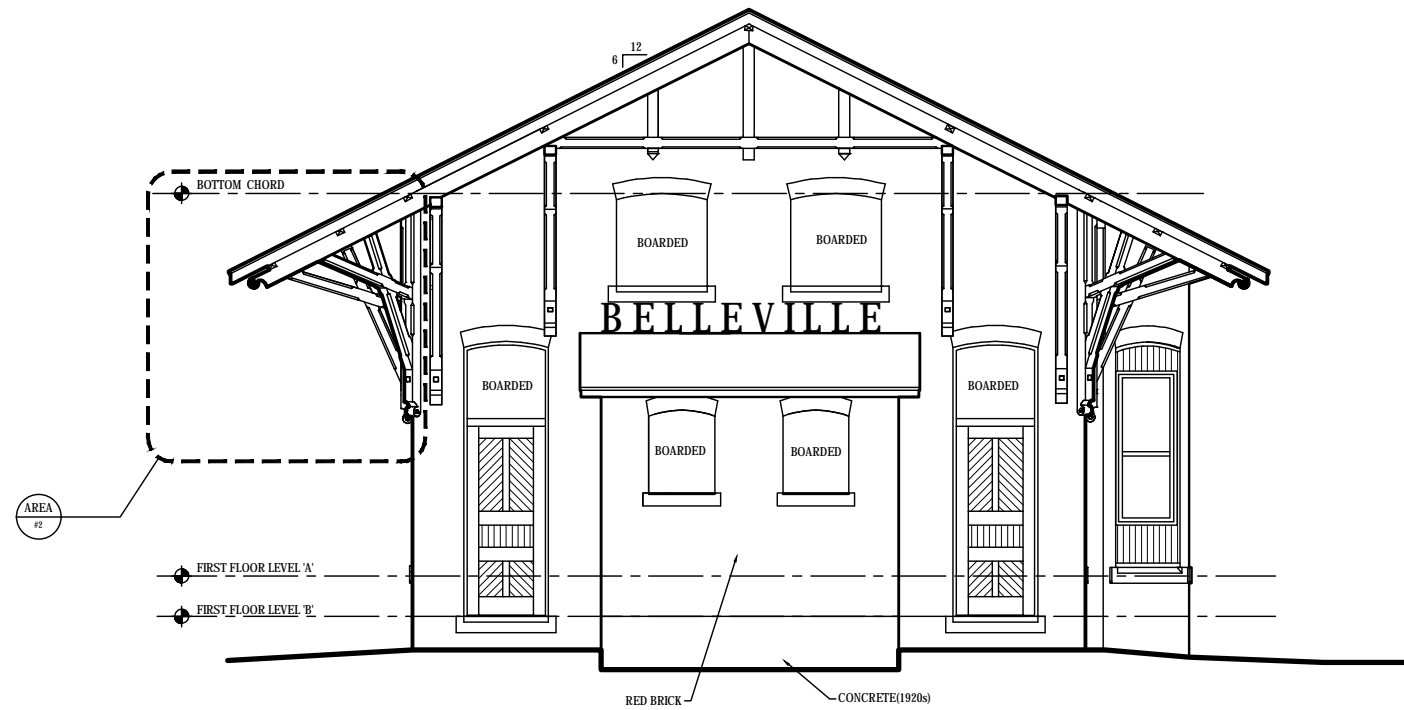
2 EXISTING REAR ELEVATION
1/8" = 1'-0"



ROOF MAINTENANCE HAS BEEN NEGLECTED- WATER HAS BEEN LEAKING INTO THE SOFFIT AND HAS ROTTED THE EXISTING WOOD BREADBOARD THIS IS A TYPICAL PROBLEM AROUND THE STRUCTURE

ROOF MAINTENANCE HAS BEEN NEGLECTED- LEAKING WATER HAS STARTED TO COMPROMISE THE STRUCTURAL BRACKETS

AREA # 2- ROOF OVERHANG CONDITION



1 EXISTING FRONT ELEVATION
1/8" = 1'-0"

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ROOF MAINTENANCE HAS BEEN NEGLECTED- WATER HAS BEEN LEAKING INTO THE SOFFIT AND WALL



AREA #5

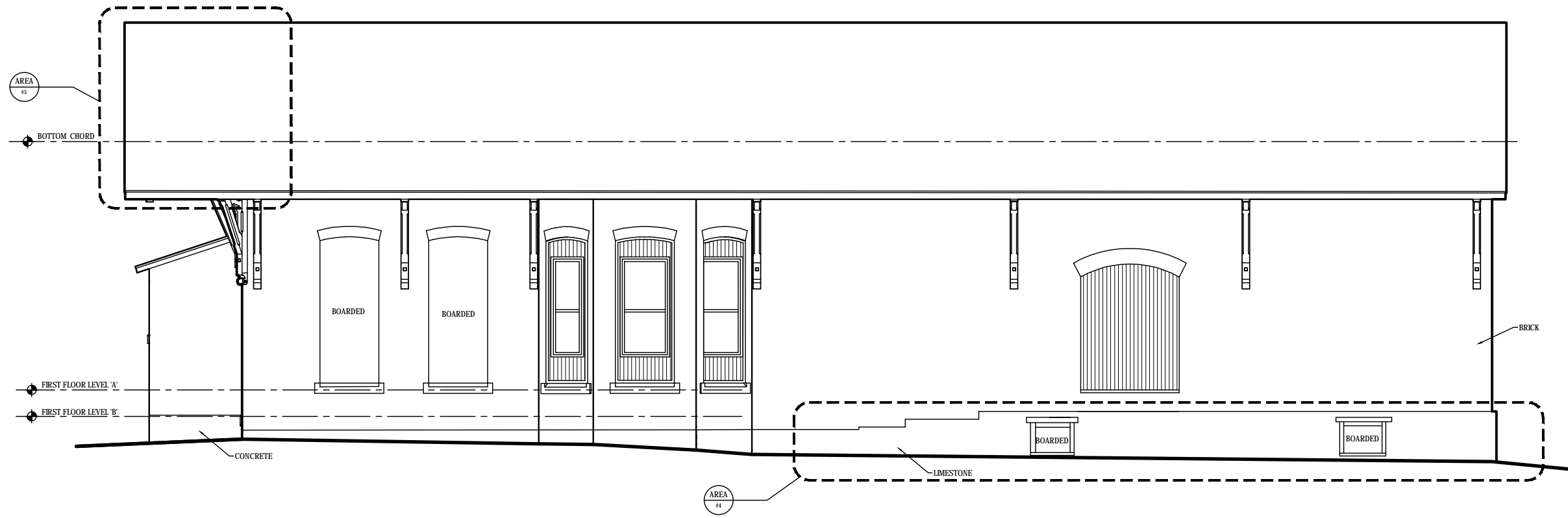


INCORRECT MORTAR USED FOR REPAIRS

EXISTING WINDOW FRAME- NEEDS RESTORATION

STONE FOUNDATION IS NEED OF TUCKPOINTING

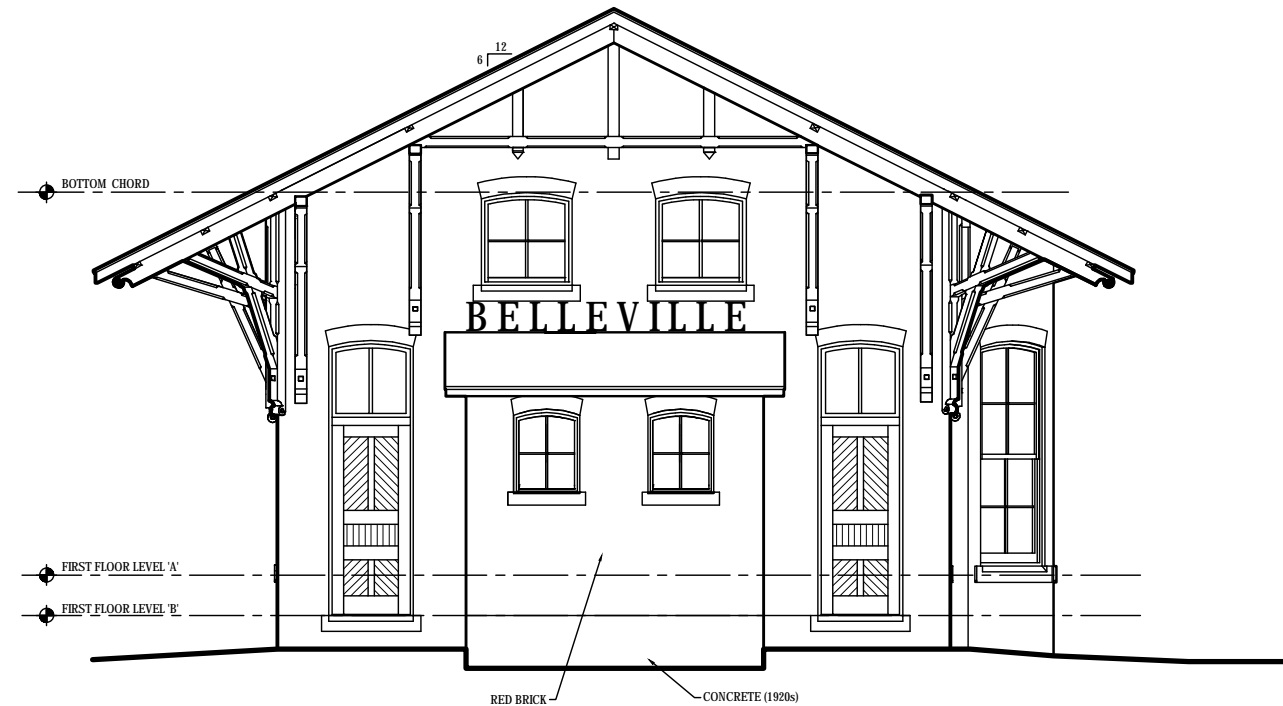
AREA #4



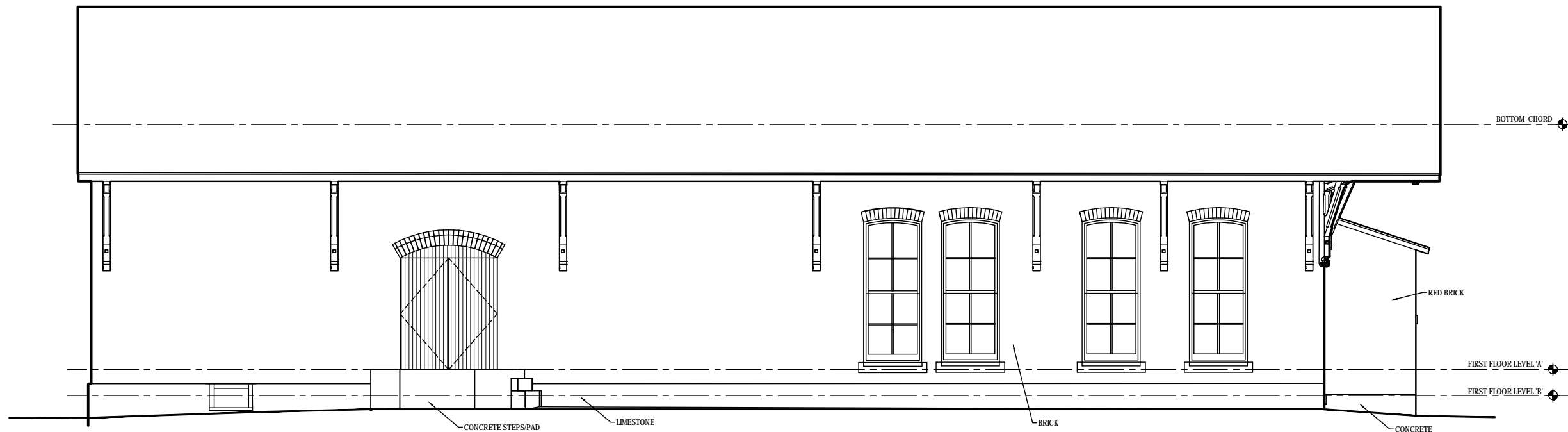
1 EXISTING SIDE ELEVATION
1/8" = 1'-0"

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2 HISTORIC FRONT ELEVATION
1/8" = 1'-0"



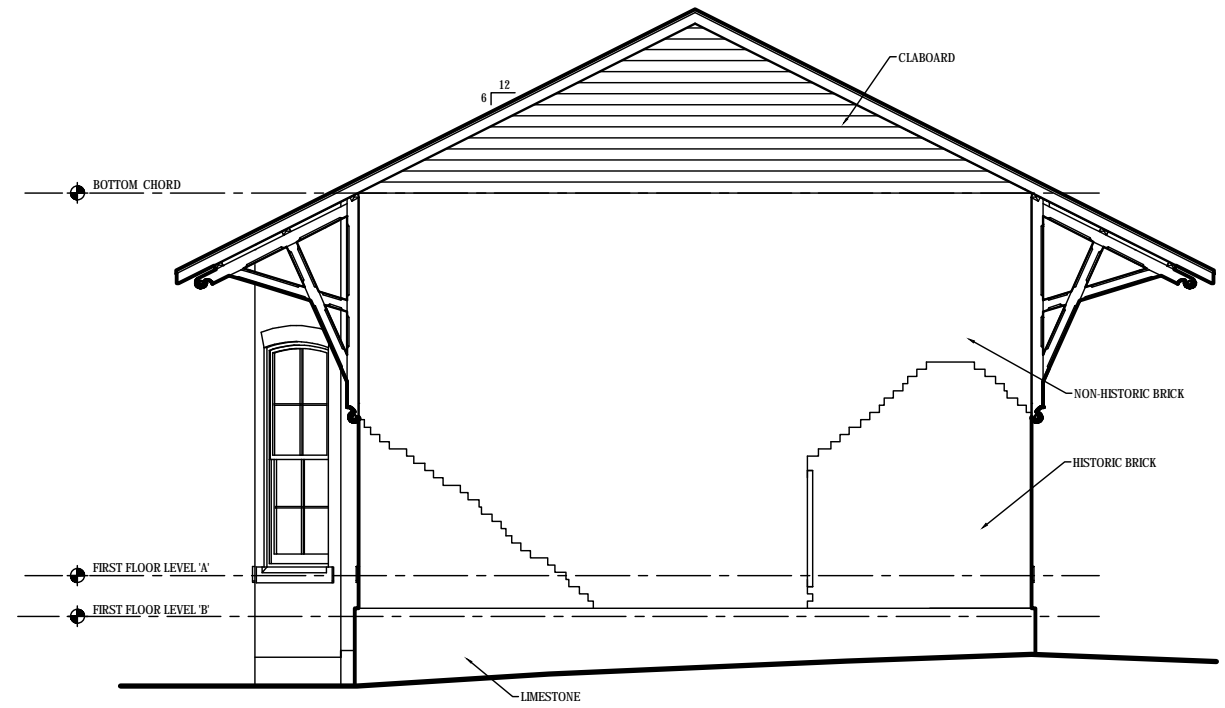
1 HISTORIC SIDE ELEVATION
1/8" = 1'-0"

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2 HISTORIC REAR ELEVATION
1/8" = 1'-0"

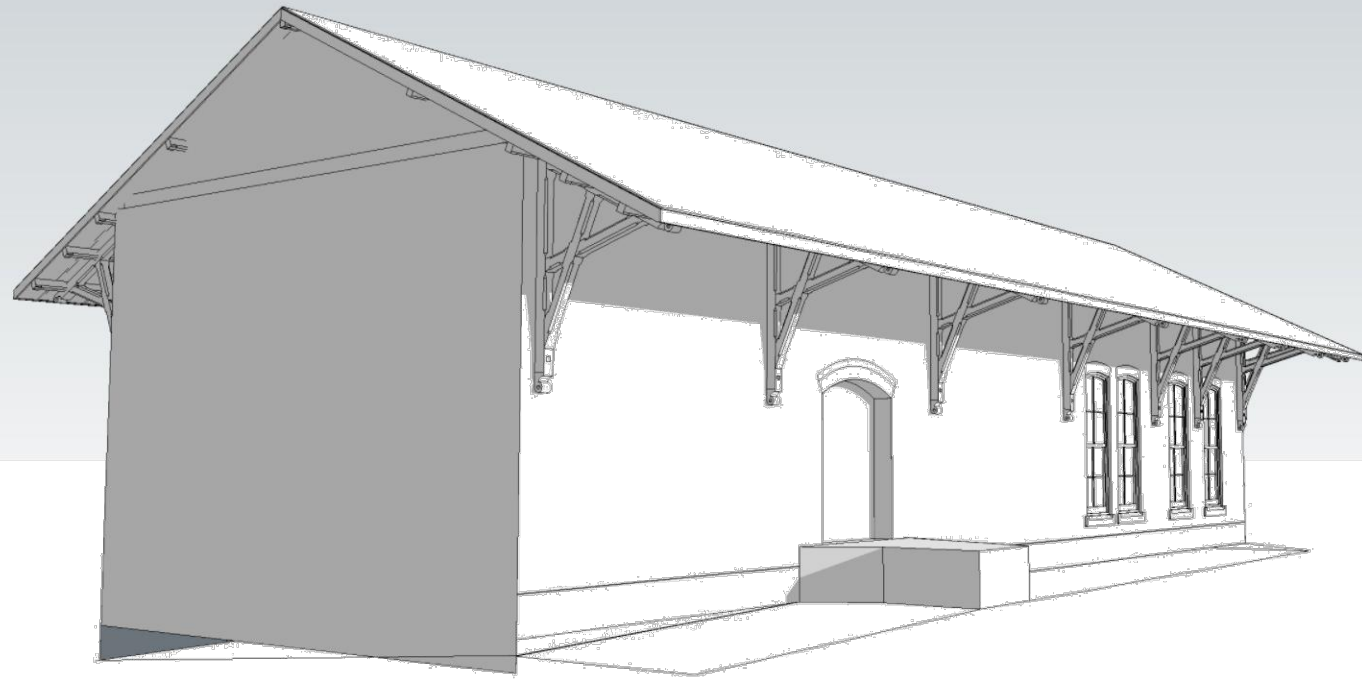


1 HISTORIC SIDE ELEVATION
1/8" = 1'-0"

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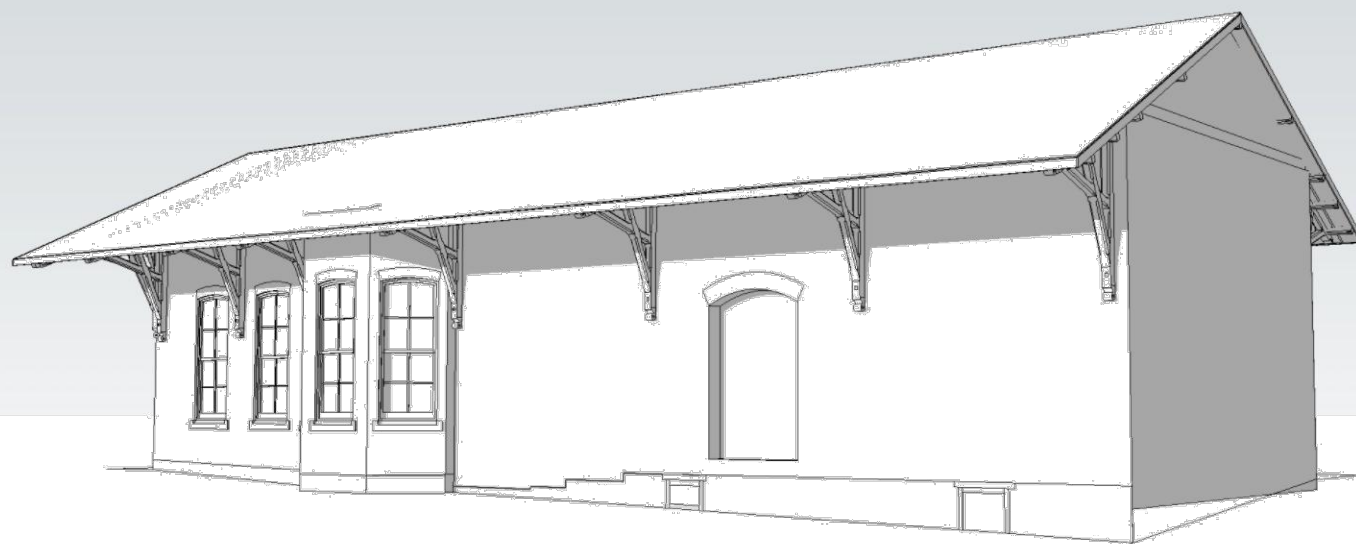
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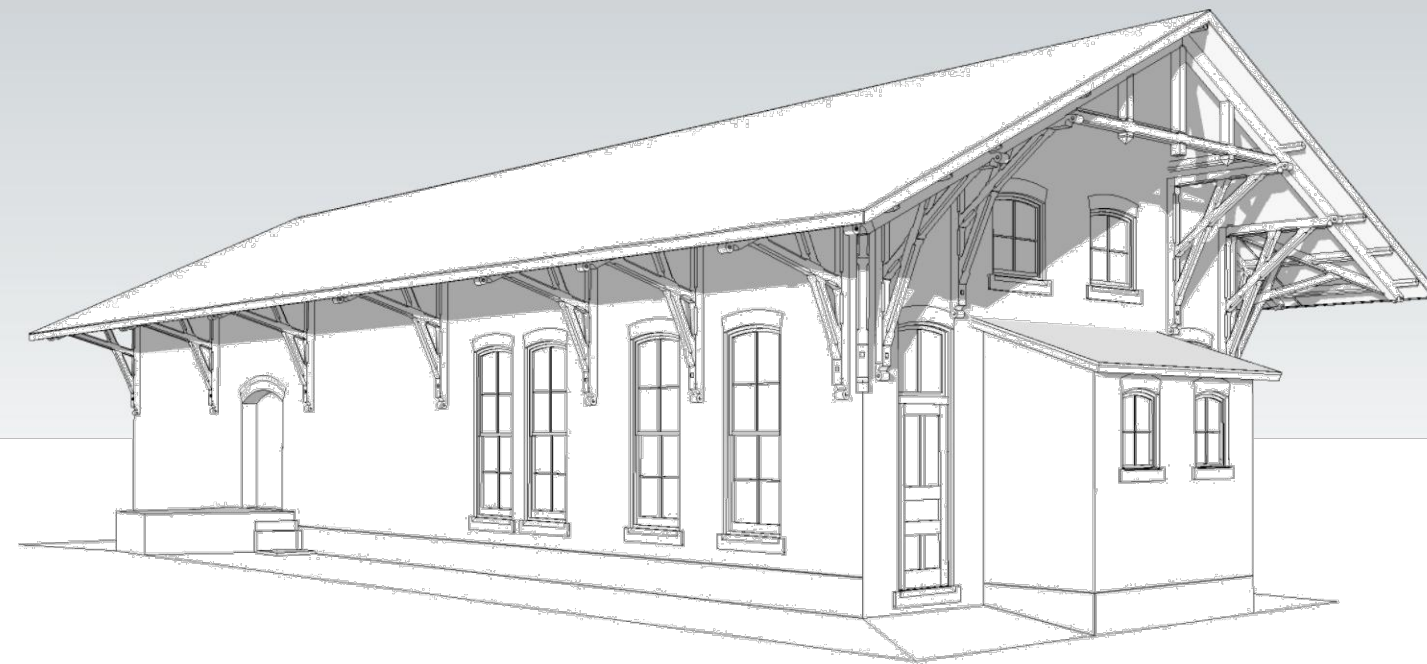
4 SOUTHEAST VIEW



2 NORTHWEST VIEW



3 SOUTHWEST VIEW

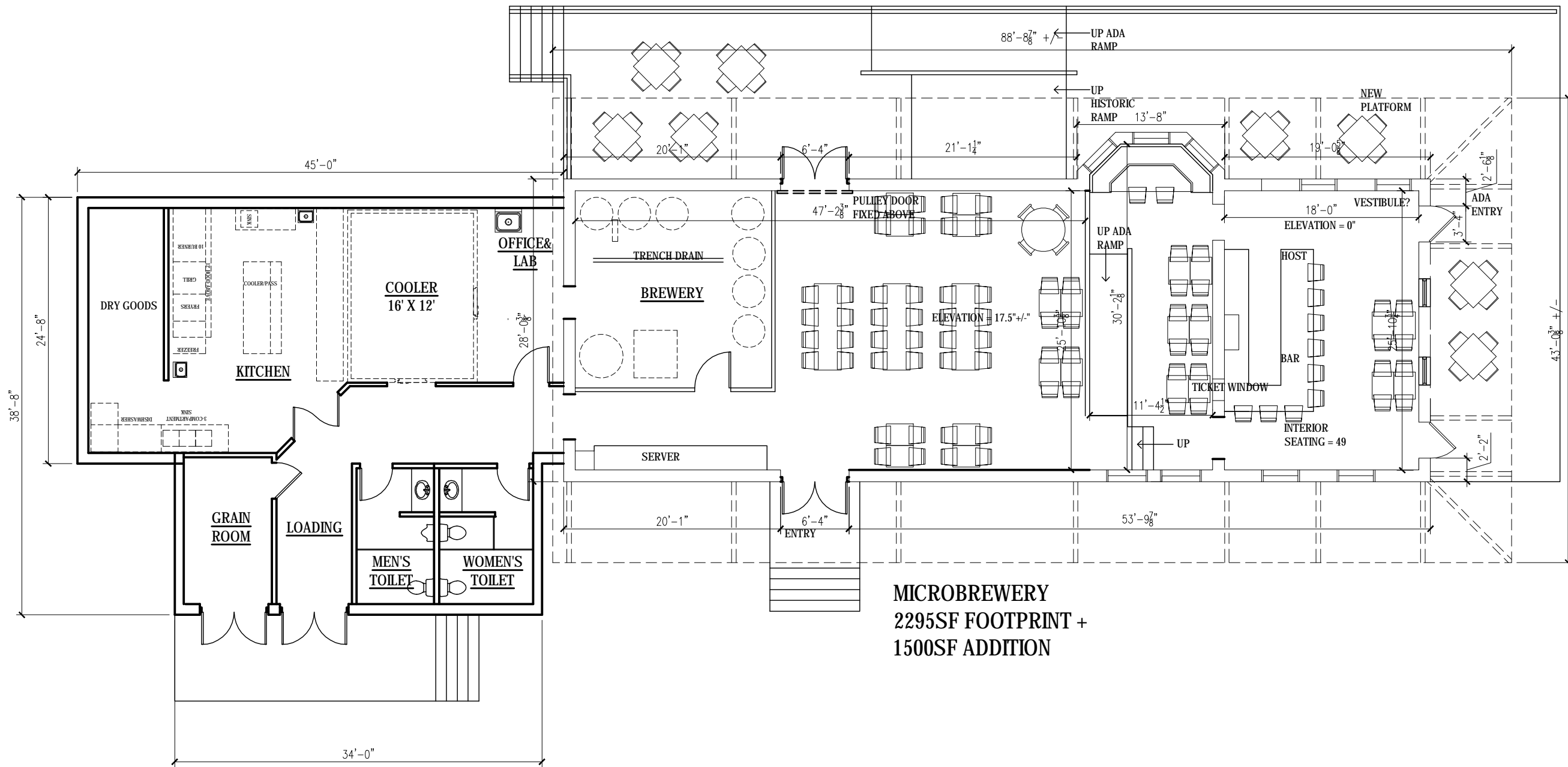


1 NORTHEAST VIEW

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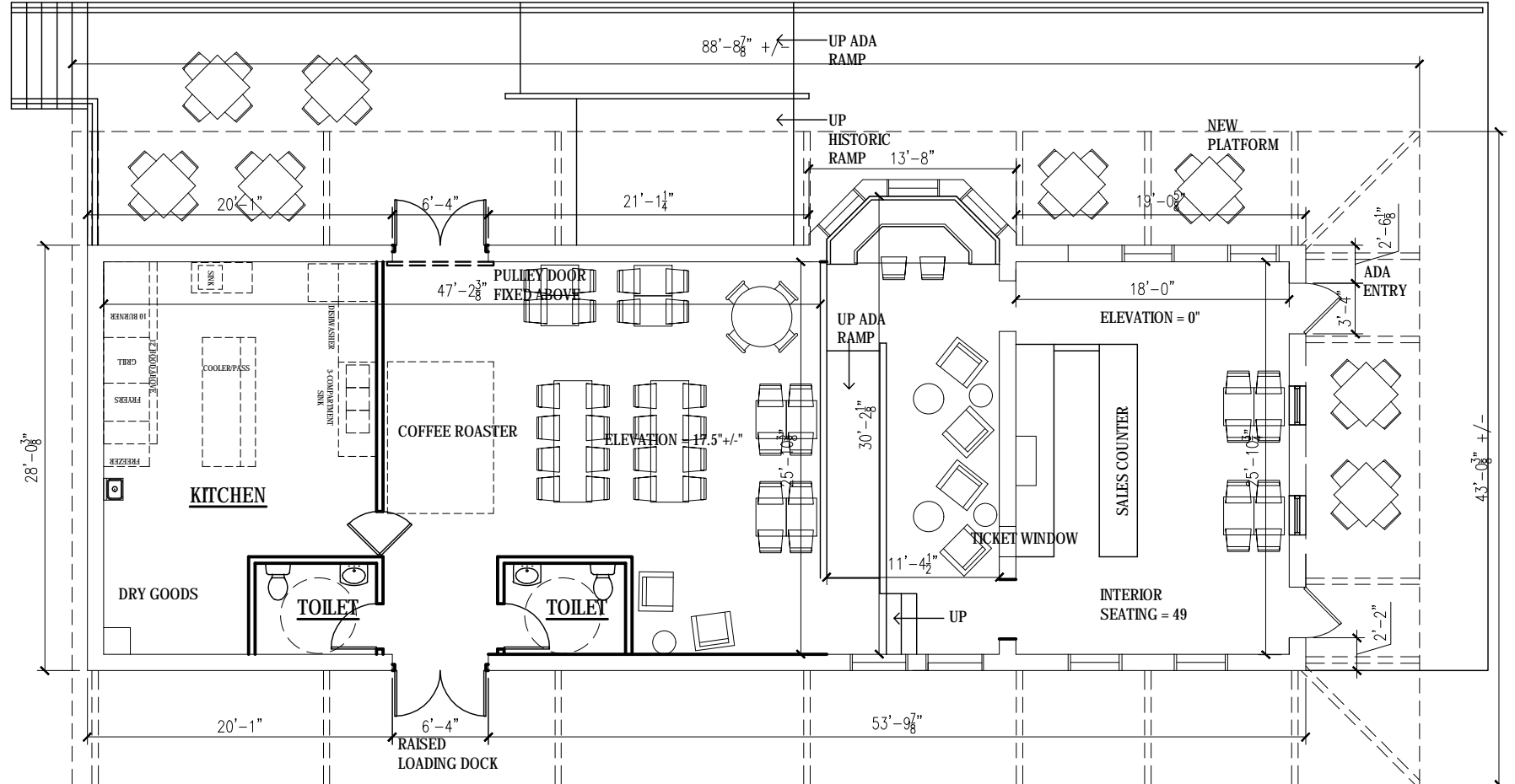
MICROBREWERY
 2295SF FOOTPRINT +
 1500SF ADDITION

1 PROPOSED PLAN W/ BREWERY
 3/32" = 1'-0"

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COFFEE SHOP/CAFE WITH ROASTER

1 PROPOSED PLAN W/ CAFE & ROASTER
 3/32" = 1'-0"

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